

COA # 2010-COA- 248 (IURS)	INDIANAPOLIS HISTORIC PRESERVATION COMMISSION STAFF REPORT	Hearing Date JULY 7, 2010
123 W. LOUISIANA ST. UNION STATION		NEW CASE
Applicant CROWNE PLAZA AT UNION STATION mailing address: 123 W. Louisiana St. Indianapolis, IN 46225 Owner: B&D Associates, L.P. 2501 S High School Rd. Indianapolis, IN 46241		Center Twp. Council District 19 Dane Mahern
CASE		
IHPC COA: 2010-COA-248 (IURS) Construct an enclosed pedestrian connector at the 2 nd level between Crowne Plaza Hotel and the Convention Center		
STAFF RECOMMENDATION: Approval		
STAFF COMMENTS		

Background

The Indianapolis Union Railway Station plan was adopted in November 1979. The boundaries of the district include the building and Capitol Avenue immediately west of the station. Union Station head house was designed by Thomas Rodd in the Romanesque Revival style. It was constructed between 1886 and 1888 and today serves as office and commercial space. The train shed was added in 1916-22 and was designed by Price and McLanahan architects of Philadelphia. It is a 2-story building with buff brick and polychromatic terra-cotta organic relief panels on the exterior that are excellent examples of Art Nouveau architectural design in Indianapolis. Today, the western end of the train shed functions as the Crowne Plaza Hotel with guest rooms and banquet facilities.

Design of the Addition

The applicant proposes construction of a metal and glass pedestrian connector over Capitol Avenue connecting Crowne Plaza at Union Station with the Convention Center.

The addition will sit on top the existing bridge. It will be approximately 10 ft in width, 77 ft in length and 9 ft. in height. It will have a rubber membrane roof and consist of an aluminum storefront style framing system in a silver finish with tinted green glass to match the glass on the convention center. The transom area above the windows will be opaque metal in a patina green color to hide the a/c ductwork in the connector and to match the terra cotta on train shed. The “cornice” will also be wrapped in the opaque metal – patina green. The interior ceiling will be lit and the glow of the light visible throughout the day.

The addition will connect to Union Station at an EIFS covered column located at the 2nd level west entry. It will not touch any historic materials on the building.

Staff finds the addition to

- not obscure, damage or destroy any character defining features;
- clearly read as a contemporary but compatible addition;
- be a minimal size and scale and located appropriately on a secondary elevation; and

- be easily removed in the future.

With this, we recommend approval of the proposal.

Preservation Plan

The IURS preservation plan states the following:

“Any development, construction, reconstruction, restoration or alteration of the subject exterior structure or site or its interior shall be appropriate to the property’s historical and architectural values and significance.”

“Any exterior or interior development, construction, reconstruction, restoration, or alteration shall be visually compatible and appropriate in function, general design, arrangement, color, texture and material to the design and character of the subject.”

STAFF RECOMMENDED MOTION

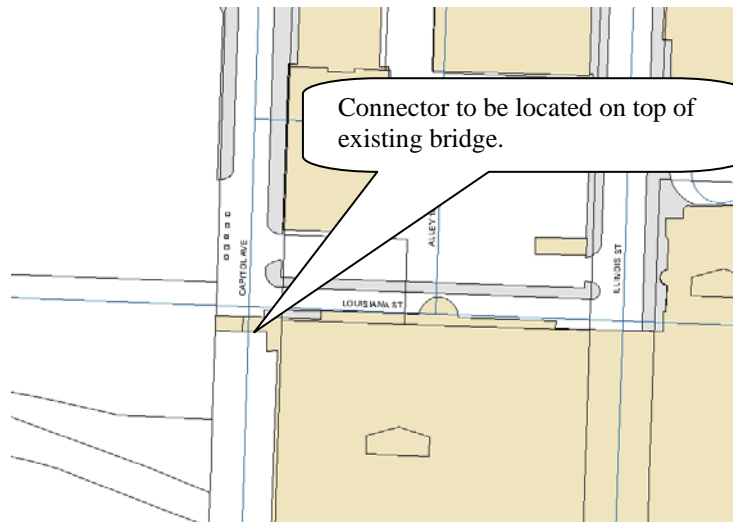
2010-COA-248 (IURS):

To approve a Certificate of Appropriateness for construction of a pedestrian connector between Crowne Plaza at Union Station and the Indiana Convention Center of per submitted documentation and subject to the following stipulations:

- 1. Construction must not commence prior to approval by the IHPC staff of final site plan and construction drawings. *Approved: _____ Date: _____***
- 2. A pre-construction meeting with IHPC staff, the designer, the owner, and the contractor/construction manager must be held prior to commencement of construction. *Approved: _____ Date: _____***
- 3. The construction shall be field staked/painted and reviewed by IHPC staff prior to the commencement of construction. *Approved: _____ Date: _____***
- 4. Work on exterior details must not commence prior to the approval by the IHPC staff of each. These may include, but are not limited to all finish material for: doors, windows, foundations, exterior light fixtures, paint colors, etc.**

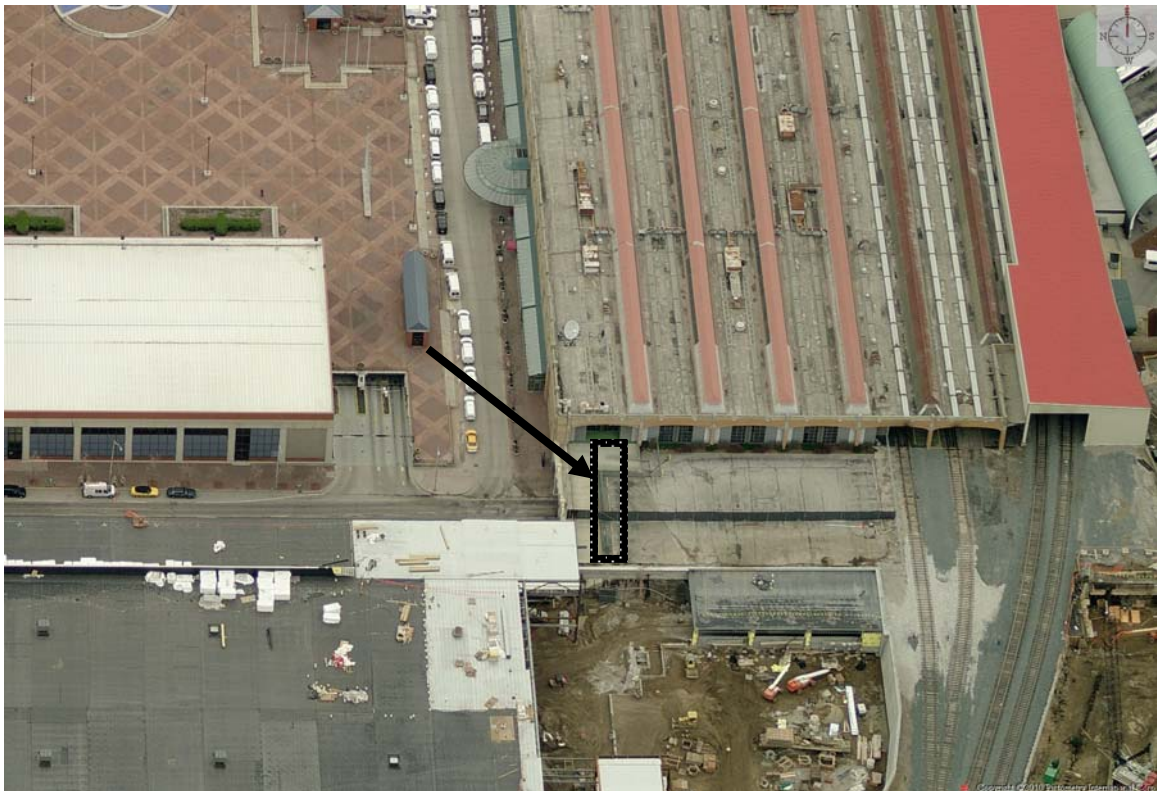
Note #1: Stipulations 1, 2 and 3 must be satisfied prior to the issuance of any building permits.

Staff Reviewer: Amy L. Bear

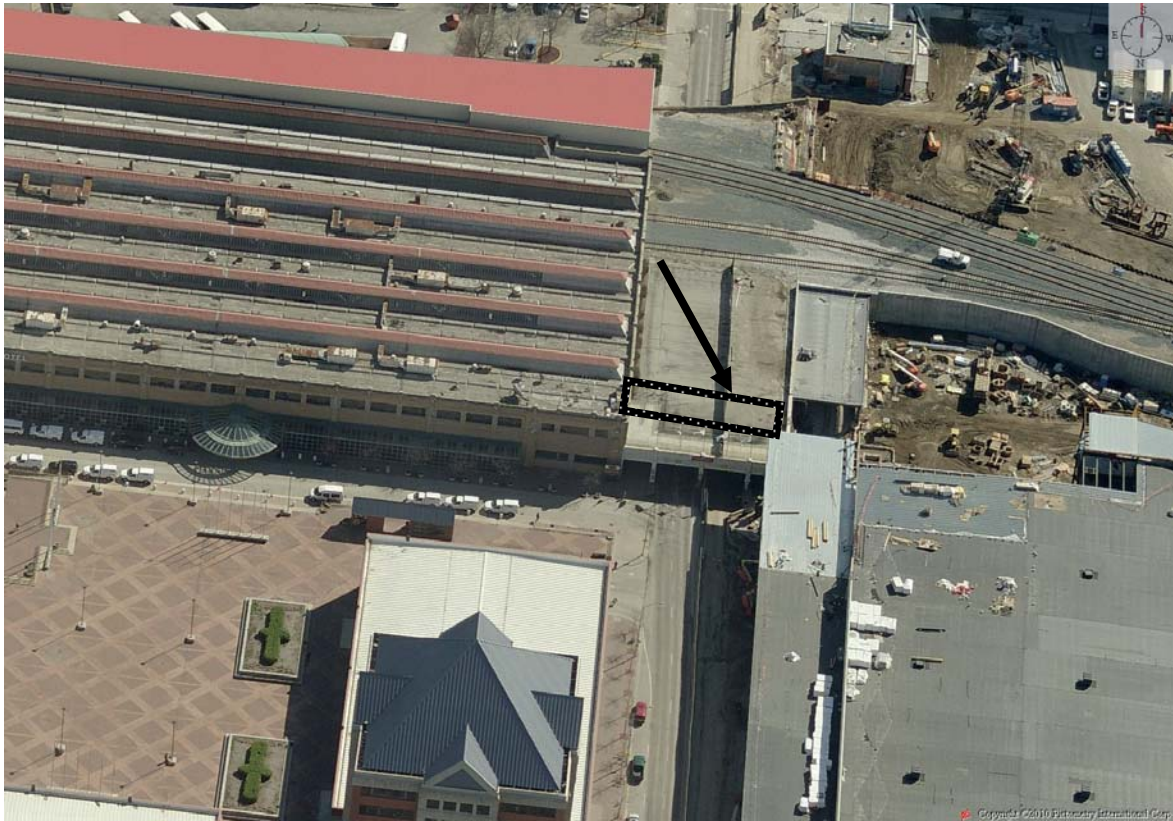




Looking North – showing location of proposed pedestrian connector



Looking East – showing location of proposed pedestrian connector



Looking South – showing location of proposed pedestrian connector



Looking south on Capitol Avenue – View of the bridge on which the pedestrian connector will be built atop.



The pedestrian connector will be located under the first arch viewable in this photo.

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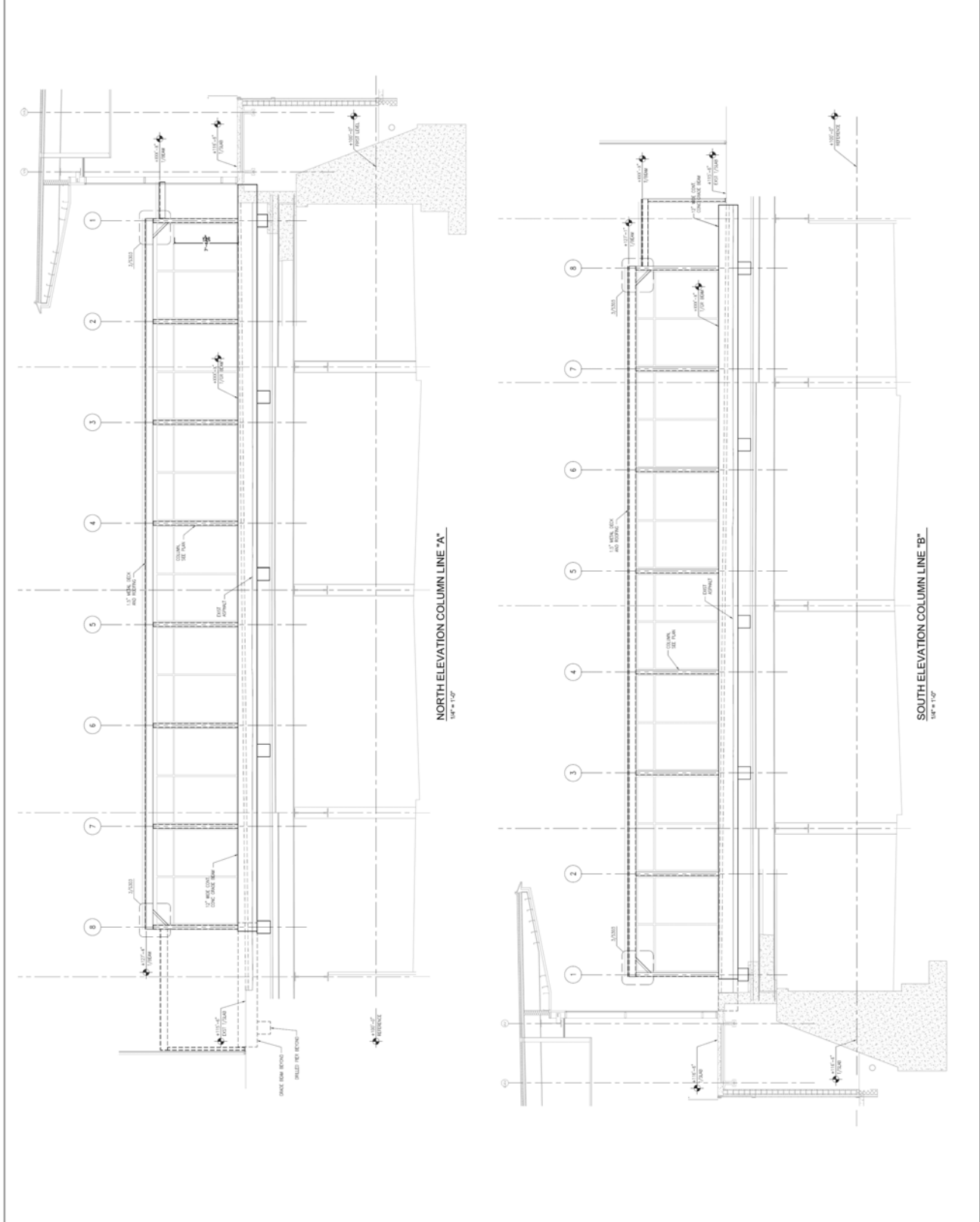
DRAWN BY: _____
CHECKED BY: _____

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Crowne Plaza Pedestrian Connector Bridge

NO.	DATE	DESCRIPTION

DATE: 01/20/10
PROJECT NO.: S302
SHEET NO.: S302



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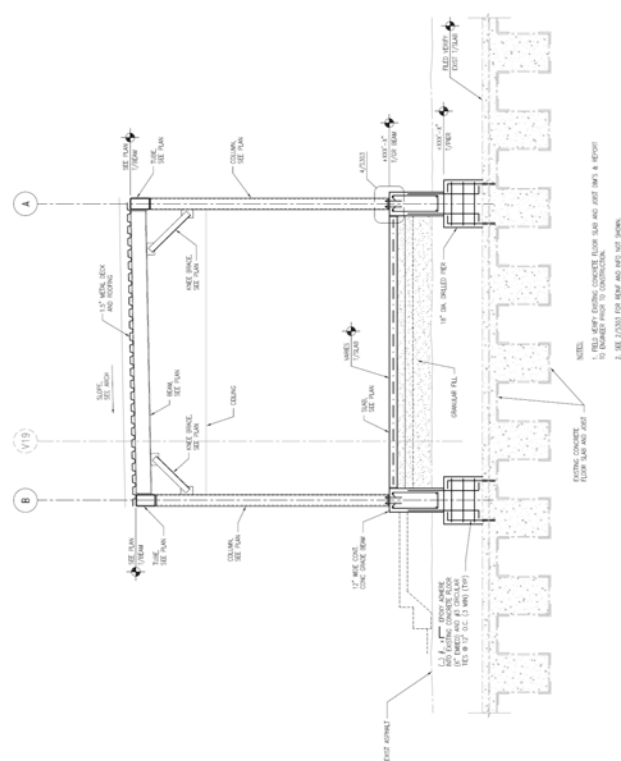
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**Crone Plaza Pedestrian
Connector Bridge**

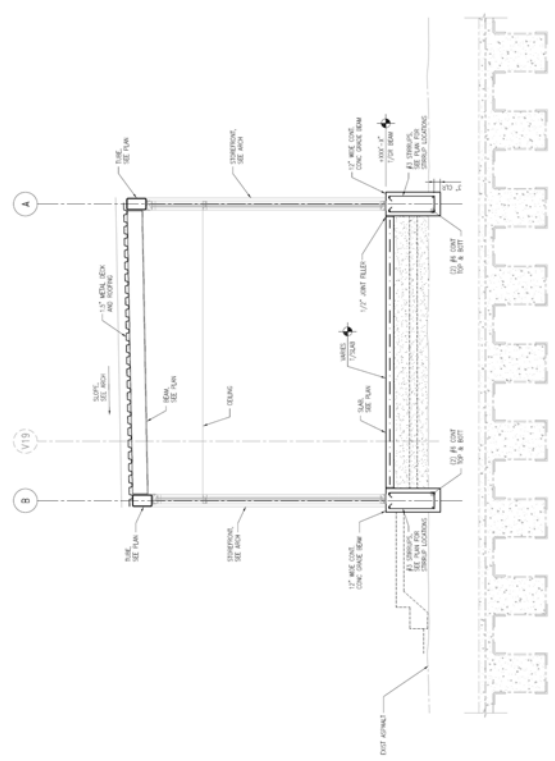
NO.	DATE	REVISIONS	DESCRIPTION

TITLE SECTIONS

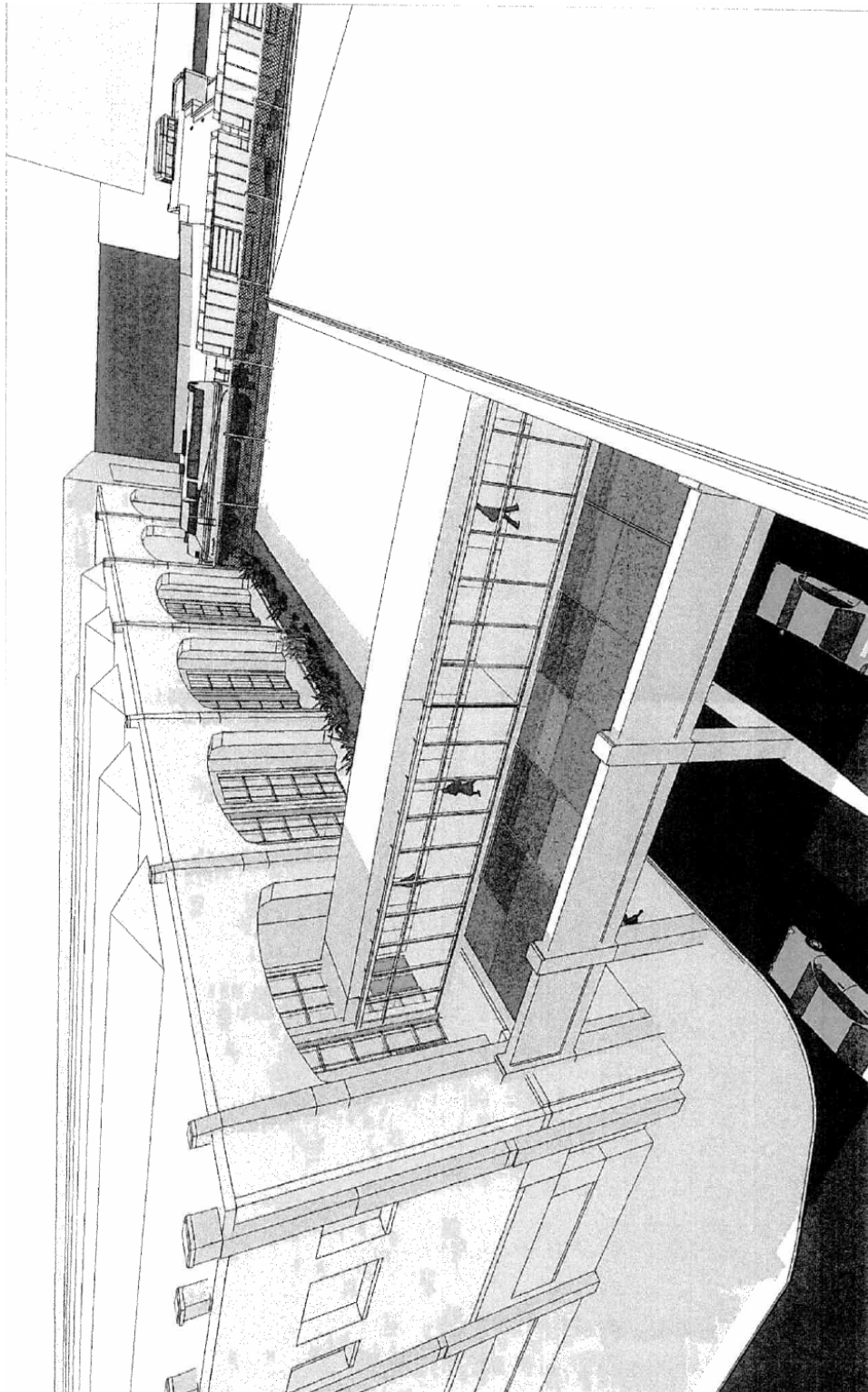
DATE: 2/10/2019
SHEET NO. S303
PROJ. NO. 1010



SECTION 1
SCALE: 1/2" = 1'-0"



SECTION 2
SCALE: 1/2" = 1'-0"



Early rendering of the pedestrian connector – transom areas will be opaque, the actual height will be slightly taller and there will be an offset in the plan to accommodate the offset entries on the two buildings.